

The Bridge Boulevard Village Center and Corridor Plan

an Addendum to the Isleta Boulevard and Village Centers Sector Development Plan

Final Version with Adopting Resolution: December, 2010



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Acknowledgements

We acknowledge and thank the following individuals and organizations for their participation and contributions during the planning process and the development of this plan.

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BERNALILLO COUNTY

BOARD OF COUNTY COMMISSIONERS

RESOLUTION NO. 2010-84

**ADOPTING THE BRIDGE BOULEVARD VILLAGE CENTER AND CORRIDOR
PLAN AN ADDENDUM TO THE ISLETA BOULEVARD AND VILLAGE CENTERS
SECTOR DEVELOPMENT PLAN.**

1 **WHEREAS**, the Board of County Commissioners is authorized to adopt Sector Plans and
2 zoning of property to protect the public well being, and the health, safety and welfare in areas
3 within Bernalillo County; and,

4 **WHEREAS**, the Board of County Commissioners adopted the Isleta Boulevard and
5 Village Centers Sector Development Plan on June 24, 2008; and,

6 **WHEREAS**, the County initiated the development of the Bridge Boulevard Village
7 Center and Corridor Plan as an addendum to the Isleta Boulevard and Village Centers Sector
8 Development Plan to guide quality development in an area that is semi rural and urban in nature
9 and to provide a mixture of uses more suitable in a quickly developing County area; and,

1 **WHEREAS**, there is a desire to protect and improve the semi rural and urban
2 characteristics and land use patterns along Bridge Boulevard which include single family
3 residential areas, commercial areas and medium to high density residential zoning; and,

4 **WHEREAS**, Sector Development Plan Zoning is appropriate for Bridge Boulevard
5 because the requirements of other available zones do not promote the conservation of the desired
6 neighborhood characteristics; and,

7 **WHEREAS**, Bernalillo County recognizes that there are changed community conditions
8 including the widening of Isleta Boulevard that justify the need for this Sector Plan addendum;
9 and,

10 **WHEREAS**, this addendum establishes zoning and design criteria for commercial and
11 residential zoning along Bridge Boulevard, thereby making Village Center zoning available to
12 parcels within the Five Points and Goff Village center boundaries if subsequently applied for by
13 property owners.

14 **WHEREAS**, the Bridge Boulevard and Village Center and Corridor Plan has been
15 reviewed by the County Planning Commission and is transmitted with their recommendation for

1 guide and govern all development actions, both public and private within the Boulevard and
2 Village Center and Corridor Plan area.

3 **SECTION TWO:** A Design Overlay Zone for Bridge Boulevard is hereby established, creating
4 signage, lighting, and design standards within the Plan area.

5 **SECTION THREE. Severability Clause**

6 If any section, paragraph, sentence, clause word or phase of this resolution is for any reason held
7 to be invalid or unenforceable by any court of competent jurisdiction, such decision shall not
8 affect the validity of the remaining provisions of this resolution. The Board of County
9 Commissioners hereby declares that it would have passed this resolution and each section,
10 paragraph, sentence, clause, word or phase thereof irrespective of provisions being declared
11 unconstitutional or otherwise invalid.

12 **PASSED, ADOPTED, APPROVED AND SIGNED THIS 26TH DAY OF OCTOBER 2010.**

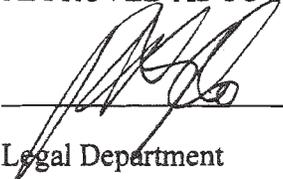
13 **BOARD OF COUNTY COMMISSIONERS**

14 
15 Art De La Cruz, Chair


1

2 APPROVED AS TO FORM:

3



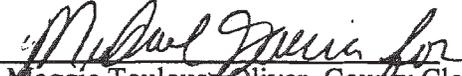
4

Legal Department

5

6 ATTEST:

7



Maggie Toulouse Oliver, County Clerk



8 THIS ORDINANCE WILL BECOME EFFECTIVE ON: _____

BOARD OF COUNTY COMMISSIONERS
OCTOBER 26, 2010

BOARD OF COUNTY COMMISSION ORDINANCE NO. 2010-21

AMENDING THE ZONE MAP OF THE BERNALILLO COUNTY AS SHOWN IN
ORDINANCE NO. 213 APPENDIX A, AS AMENDED.

BE IT ORDAINED BY THE BOARD OF COUNTY COMMISSIONERS.

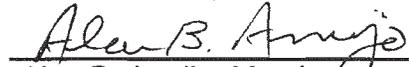
The official Bernalillo County zone map is amended to include the zoning
and design overlay language as called out in Section 7 of the Bridge
Boulevard Village Center and Corridor Plan.

PASSED, ADOPTED, APPROVED AND SIGNED THIS 26th DAY OF
OCTOBER 2010

BOARD OF COUNTY COMMISSIONERS
COUNTY OF BERNALILLO



Art De La Cruz, Chair



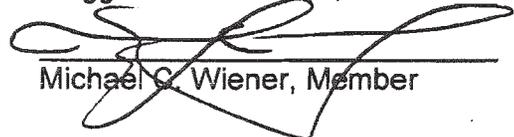
Alan B. Armijo, Member



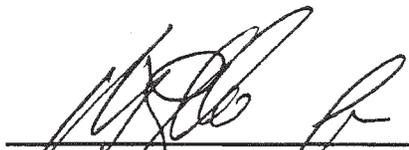
Michael Brasher, Member



Maggie Hart Stebbins, Vice-Chair



Michael C. Wiener, Member



Jeffrey Landers, County Attorney

ATTEST:



Maggie Toulouse Oliver, County Clerk



ADOPTING ORDINANCE

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Bridge Boulevard is an historic corridor that carries more than 30,000 cars per day across the Rio Grande and through the heart of the South Valley. In response to concerns about safety and building conditions along the corridor, Bernalillo County Commissioner Art De La Cruz initiated this project to create a corridor plan for Bridge Boulevard, from the Eastside Drain to Goff Avenue. The Bridge Boulevard Village Center and Corridor Plan (Corridor Plan) is an addendum to the Isleta Boulevard and Village Centers Sector Development Plan. The purpose of the Corridor Plan is to promote safety, spur economic development, and celebrate the historic character of the corridor through a comprehensive set of standards and guidelines for both the street itself and adjacent properties.

Bridge Boulevard today is a mix of small-scale commercial uses mixed with long, narrow residential lots that, in some cases, still connect to acequias and agricultural operations. Despite the large volume of traffic, commercial businesses struggle to remain viable. Traffic congestion is among the worst in the region and the corridor had over 300 accidents from 2005-2007.

At the outset of this project, the County hosted public forums to solicit input and discuss ideas on how to improve the area. On April 21, 2010 the County held a forum at the South Valley Economic Development Center. The County hosted a second forum at the Westside Community Center on June 15, 2010. Adjacent property owners were notified by certified letters. Both forums were well attended and resulted in a positive dialogue about the corridor.

The overall purpose is to promote safety, spur economic development, and celebrate the historic character of the corridor through a set of guidelines and standards for new and existing development.



The Corridor Plan has two major components: one set of standards that address the Bridge Boulevard right-of-way and one set of standards that apply to parcels of land abutting Bridge Boulevard. The proposed improvements to the public right-of-way will be implemented by the County. The design standards for properties adjacent to Bridge will be administered by the County as part of typical site plan reviews. They will apply to new construction and renovation projects that impact 200 square feet or more of an existing structure.

The proposed standards for the public right-of-way include the following:

- Improved crosswalks and pedestrian safety
- More contiguous sidewalks with fewer curb cuts
- Cohesive landscaping along medians, sidewalks, and parking lots
- Gateway features at the east and west ends of the corridor
- Street lighting that reflects the historic character of the area
- Screening of mechanical equipment on roofs and in yards
- Color, signage, and wall standards for new and existing development

To spur investment in the corridor, the County is proposing economic incentives for properties along Bridge Boulevard. These include possible property tax abatements, impact fee incentives, and high wage job tax credits.

Concept rendering showing commercial development that engages the street

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Corridor Plan Boundary



1 Corridor Plan Boundary

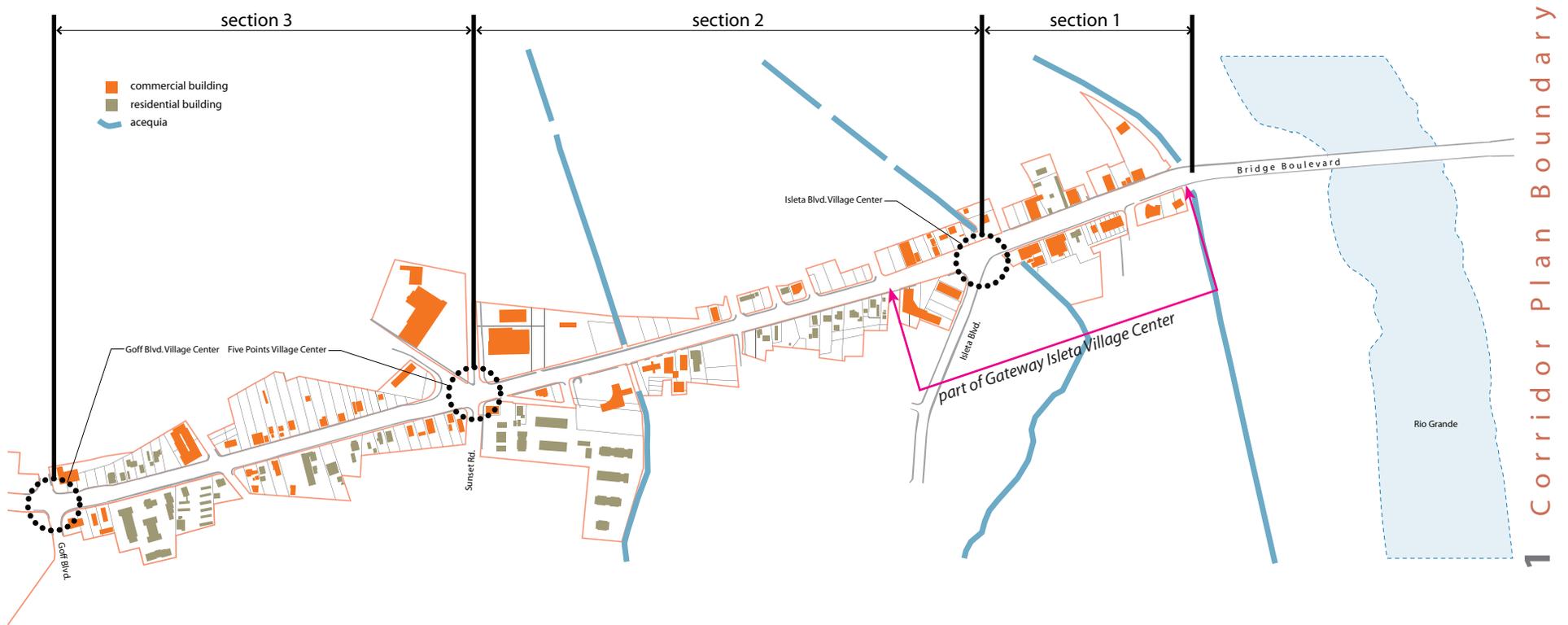
Area map of Southwestern Albuquerque showing the central location of Bridge Boulevard relative to downtown, the University, Central Avenue, the original Route 66 and the greater South Valley and West Side.



Bridge Boulevard Corridor Plan Boundary

The Bridge Boulevard Corridor Plan covers the area from the west side of the Rio Grande crossing to Goff Blvd, a distance of about one mile. The Corridor Plan applies to all parcels that abut Bridge Boulevard and are outside of the Albuquerque municipal limits. As an addendum to the Isleta Boulevard and Village Centers Sector Development Plan, this Corridor Plan keeps the current zoning designations intact and does not propose any modifications to them.

The Isleta Boulevard Sector Development Village Center zone (SD-IVC) still applies to parcels from the East side Drain to Hartline Road. The only difference is that those parcels, like others that abut Bridge Boulevard, will be subject to the design guidelines and requirements detailed in this document.



There are three sections to the corridor:

1 Rio Grande to Isleta:	1,000 ft. (approx.)
2 Isleta to Five Points:	2,640 ft. (approx.)
3 Five Points to Goff Blvd.	2,100 ft. (approx.)

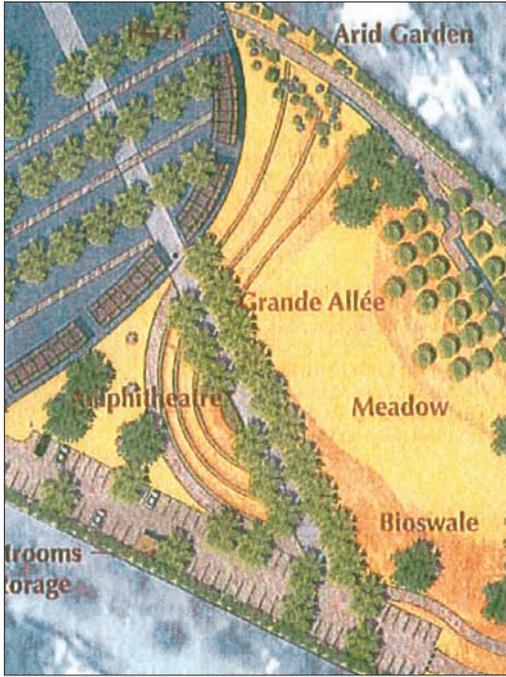
Each section has its own character, opportunities, and constraints. As shown on the exhibit above, each section also has its own designated Village Center that is expected to act as a hub for that portion of the corridor.

The exhibit also distinguishes between residential versus commercial buildings. The distinction is more to distinguish the scale of the buildings versus the use of the buildings. In other words, some of the buildings classified as residential buildings may have been converted to commercial uses. However, the commercial zone allows residential uses.

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Existing Conditions





SW Gateway Park Concept Plan, developed by Bohannon Huston Inc.



Isleta Boulevard south of Bridge showing landscape buffer to street.



Example of existing development on Bridge Blvd.



Example of existing development on Bridge Blvd.

Existing Conditions

Historically the first crossing of the Rio Grande in Albuquerque, Bridge Boulevard today reflects both the agricultural legacy of the area, as seen in the long, narrow lots that still have residential structures, and the commercial corridor legacy as evidenced by the preponderance of auto-oriented establishments. Until 1937, the eastern portion of Bridge Boulevard was part of Route 66, one of the first automobile routes across the western part of the United States. With the realignment of the “Mother Road” to Central Avenue, Bridge Boulevard became more of a local corridor serving the commercial needs of the South Valley. Like many auto-oriented streets, businesses along Bridge in the latter part of the 20th century witnessed a slow decline as larger retail centers drew larger shares of business, resulting in the closure of many neighborhood-scale establishments. Yet the volume of traffic has

continued to grow as Bridge serves as a link between new growth on the westside of Albuquerque and the established core on the eastside.

In 2010, Bridge Boulevard ranks as the fourth most congested corridor in the region. From 2005 -2007, over 300 vehicular accidents were reported on Bridge Boulevard (MRCOG data). Current zoning along the corridor is mostly C-1, Neighborhood Commercial. Current uses are dominated by auto services, restaurants, and miscellaneous retail. The larger retail sites struggle to retain tenants; some of the sites have been converted to flea markets that house a number of small businesses. The street lacks amenities such as landscape, continuous sidewalks, and gateways. Pedestrian crossings are not well-marked, tend to be long, and lack countdown signals. Large curb radii,

Vecinos del Bosque
neighborhood association

Bus Stops

Street Lights

Trees

Commercial Buildings

Residential Buildings

Acequias

Traffic Signals



2 Existing Conditions

free-right turn lanes, and irregular geometry at intersections also contribute to conflicts between pedestrians and vehicles. Overall, the corridor works well to carry traffic but not to serve local businesses or to encourage pedestrian movement.

There are a number of infrastructure constraints:

- A high pressure gas line runs along the south side of Bridge to Five Points.
- An electric distribution line runs along the south side of Bridge.
- A gas distribution line runs down the center of the street.
- Expansion of the existing roadway is not feasible without extensive purchase of additional property.

There are also a number of opportunities along Bridge:

- The County has designed and funded construction for the South Valley Gateway Park at the southeast corner of Bridge and Isleta (*pictured on opposite page*).
- The County has designed intersection improvements for Bridge/Isleta.
- The County has built a storm water detention basin on the north side of Bridge near Hartline Road.
- The lack of neighborhood-serving retail presents opportunities for new retail .

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Regulatory Framework And Contributing Plans



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The County is authorized to adopt Sector Plans and Corridor Plans to protect the public well being, and the health, safety, and welfare of Bernalillo County residents.

1. Bernalillo County/City of Albuquerque Comprehensive Plan (Comp Plan)

The Comp Plan characterizes Bridge Boulevard as a boundary between the Established Urban Area and the Semi-Urban Area.

i. Policy B on page II-24 of the Comp Plan states the following:

Developing Urban Areas shall be subject to special requirements for low-density holding zones to allow for sector planning, special design treatments, and phasing of infrastructure in keeping with capital improvement priorities.

ii. Policy D proposes specific techniques:

Specify development guidelines based on local environmental characteristics and community values in sector and area plans; implement through Comprehensive Development Code or traditional zoning, subdivision, and development review process- (p. II-25, Comp Plan).

2. Southwest Area Plan

The 2000 Southwest Area Plan (SWAP) covers the southwest area of the County, including the Bridge Boulevard corridor. The Plan recommends detailed transportation and land use studies be conducted on Bridge and Isleta Boulevards to help inform economic development strategies. The following are quotes from SWAP:

Two roadways within the plan area, Bridge and Isleta Boulevards, need detailed studies of existing and future land uses and existing and future roadway functions in relation to economic development opportunities. Corridor plans should examine these issues in relation to the revitalization of the adjacent land uses and efficient function of the roads (p. 47).

Certain land use strategies such as mixed-use developments, higher densities in some areas, and bicycle, pedestrian, and transit-friendly development can decrease congestion significantly by reducing the number of trips citizens need to make (p. 58).

(Village Centers) will provide for some housing of higher density, such as residences over stores, provide day-to-day services to surrounding neighborhoods, and will provide combined public and private services in centralized locations. New development proposed for the centers will be compatible in design, scale and character with the existing surroundings (p. 51).

3. Isleta Boulevard and Village Centers Sector Development Plan

The Board of County Commissioners adopted the Isleta Boulevard and Village Centers Sector Development Plan on June 24, 2008. Resolution No. 61-2008 articulates the reasons for adopting the Isleta Boulevard and Village Center Sector Development Plan. This Corridor Plan is an addendum to the Sector Plan and falls under the umbrella of that resolution.



Corridor Design Policy



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Goals and Objectives

The intent of the Corridor Plan is articulated through the following series of goals and objectives:

Land Use

Goal: Provide a range of land uses on Bridge Boulevard that will create a destination and provide opportunities for neighborhood and regional-serving retail, entertainment, and services.

- Objective LU-1:** *Encourage mixed-use development with a range of residential housing types and balanced mix of uses that will serve residents and visitors.*
- Objective LU-2:** *Create a critical mass of retail and entertainment opportunities to provide convenient services and create a destination for area residents.*
- Objective LU-3:** *Encourage pedestrian-oriented retail shops offering goods and services that serve the local residential population. Encourage a balance between independent/locally-owned business and franchise and corporate entities.*
- Objective LU-4:** *Encourage transit-oriented development with mixed uses, higher density residential uses, bus shelters and other transit amenities, and reduced parking requirements where appropriate.*

Economic Development

Goal: To encourage the development of the corridor as a retail and service destination for commuters, local residents, and visitors.

- Objective ED-1:** *Encourage the development of local businesses, with an emphasis on entertainment, dining, and resident-serving goods and services.*
- Objective ED-2:** *Actively recruit quality commercial and office tenants and consider incentives to attract these uses.*
- Objective ED-3:** *Pursue opportunities to locate State, County and other public office uses within the corridor.*
- Objective ED-4:** *Organize corridor public events, parades and festivals. The Gateway Park will be used for farmers markets, arts and crafts fairs, concerts etc.*
- Objective ED-5:** *Organize a merchants association. Prepare brochures to market corridor businesses and activities to tourists and residents.*

Urban Design

Goal: Develop a strong identity and character for the corridor through high quality architectural and streetscape design in order to foster a safe and attractive walking environment.

- Objective UD-1:** *Ensure that new development enhances the character of Bridge Boulevard by requiring design qualities and elements that are appropriate in look and scale to the local context and pedestrian orientation .*
- Objective UD-2:** *Improve streetscapes and create a sense of arrival at key gateways, including the river, Isleta Boulevard, Five Points, and Goff Blvd.*
- Objective UD-3:** *Develop appropriate public art to further establish a sense of unique identity along the corridor.*
- Objective UD-4:** *Promote a built environment that reduces the potential for crime and improves the quality of life through maintenance, natural surveillance and design.*

Corridor

Goal: Improve the appearance and function of Bridge Boulevard by creating an enhanced streetscape.

- Objective C-1:** Focus on creating an improved pedestrian environment, with continuous sidewalks on both sides of the street and high quality landscaping. Use street trees, special paving, high quality street lighting, and pedestrian furnishings to encourage pedestrian mobility.
- Objective C-2:** Improve pedestrian connections across intersections, especially at Isleta Boulevard, Five Points, and Goff Blvd. Shorten crossing distances, improve crosswalk markings and signals, and heighten driver awareness of crossings to improve pedestrian safety and comfort.
- Objective C-3:** Provide continuous bike lanes on Bridge Boulevard.
- Objective C-4:** Enhance and coordinate bus connections along Bridge Boulevard.
- Objective C-5:** Improve pedestrian connections to ditch trails crossing the corridor.

Open Space

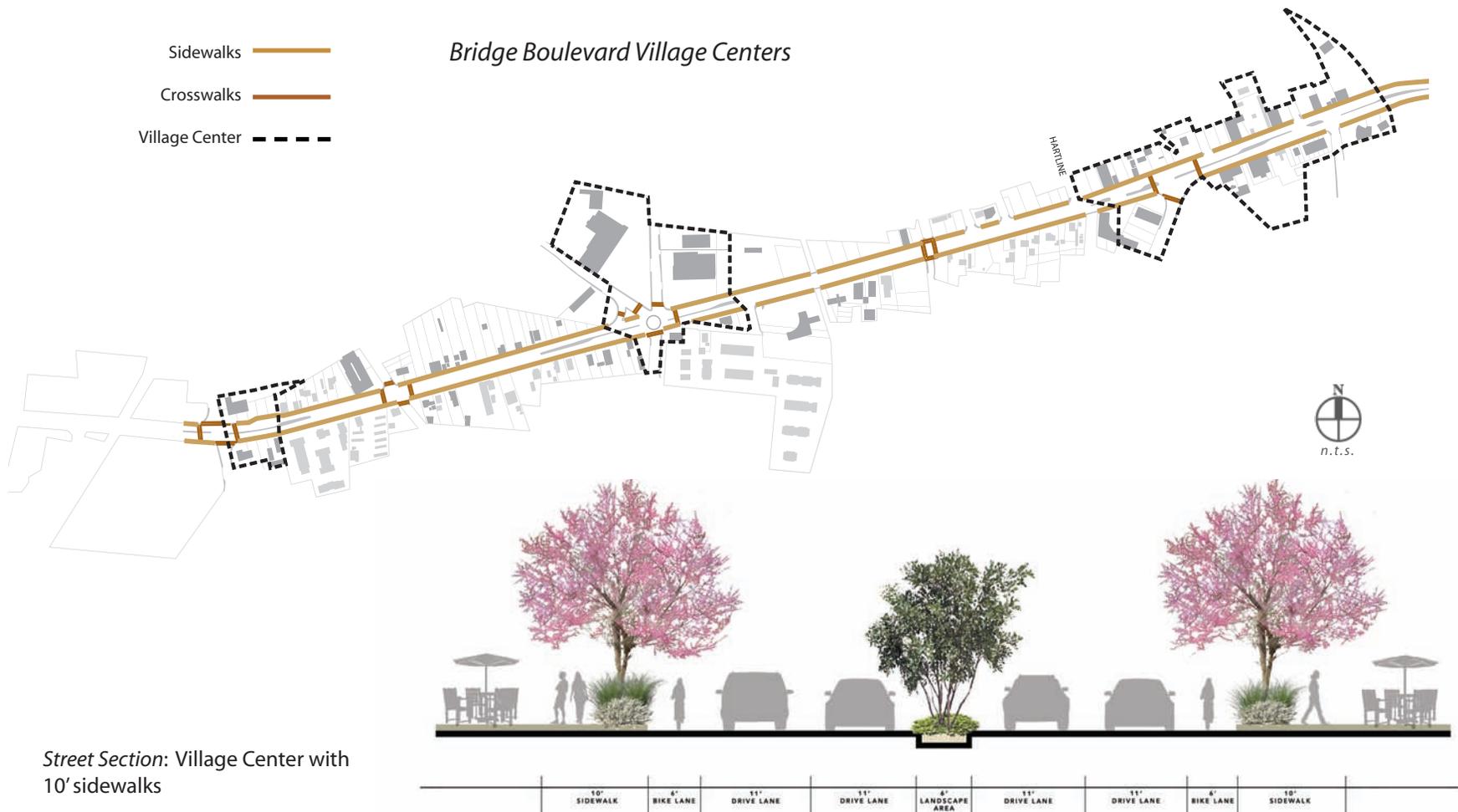
Goal: Improve the area's offering of usable open space and take advantage of the proximity to the Rio Grande.

- Objective OS-1:** Create parks or publicly available playing fields within walking distance of the corridor.
- Objective OS-2: Encourage the provision of usable open space in the form of courtyards, plazas and open space areas within new development.
- Objective OS-3: Protect and provide improved access to the Rio Grande Bosque and Gateway Park.
- Objective OS-4: Develop recreational multi-use paths in green ways along the west side of the Rio Grande.
- Objective OS-5: Create district-level storm water detention facilities serving multiple sites and that can also serve as active and passive recreation areas.

(Note: All development within the corridor shall adhere to Chapter 38 "Floods" of the Bernalillo County Code and any related information utilized by the County Floodplain Administrator to ensure adherence to this code.)



Cottonwoods lining an acequia.



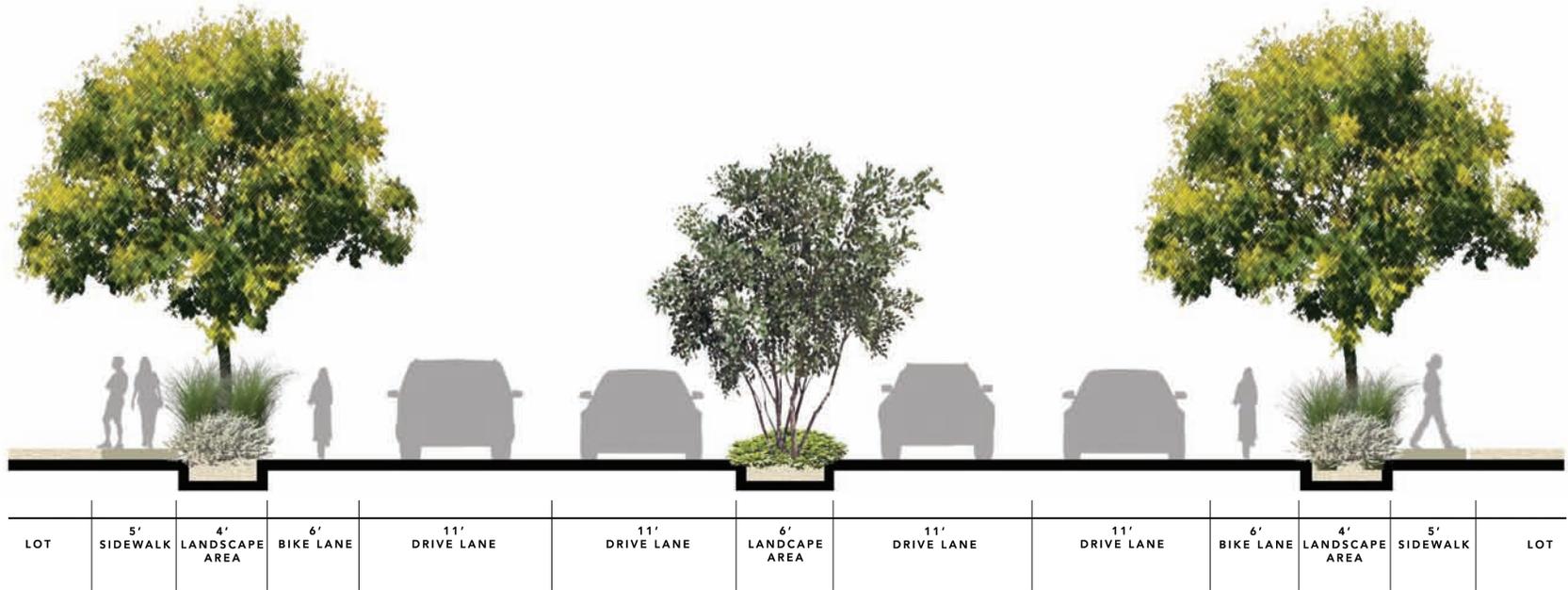
Sidewalks + Crosswalks

Sidewalks are programmed into two separate approaches:

- Corridor: Outside the Village Centers, the sidewalks will be narrower, 5 feet and separated from the street with a continuous landscape buffer. Street trees, street lights and pedestrian lights will be equally spaced and coordinated with existing utility easements.
- Nodes or Village Centers: Around the Village Centers (at Five Points and Goff) where denser commercial development is anticipated, and therefore a higher level of pedestrian traffic is expected, the sidewalk will widen to 10 feet and extend from the street curb to the private property line. In these areas the sidewalk is distinguished by decorative paving patterns or decorative pavers and is punctuated by landscape cutouts, public art and site furnishings. (See street section illustration above.)

A change in material, texture or painted pattern for the crosswalks helps alert vehicles of the crossing, and creates a safe route for pedestrians.





Street Section: 80' ROW with Bike Lanes



Street Section: 90' ROW with On Street Parking



Street trees improve the pedestrian environment by providing shade, a buffer between the street and sidewalk, and a shift in scale from adjacent buildings.



Street trees cut into the pavement along a narrow sidewalk create shade for pedestrians, and provide a wider walking area.



Landscaping along parking areas and patios helps create a comfortable environment, and helps to buffer pedestrians from street traffic.

Landscaping

The South Valley is known for its landscape – large cottonwoods along the riverbanks and ditches, small orchards, and broad, fertile fields. This bucolic landscape is in sharp contrast to the existing impression of the vehicle-oriented Bridge Boulevard corridor - with 4 lanes of traffic, overhead lines and utility poles all along the southern edge of the street. Significant, long-lasting changes in the appearance and sustainability of the corridor will require special attention to landscape design within both the public right-of-way and the private realm.

- Provide a physical separation or buffer between pedestrians on the sidewalk and vehicles in the street. Where trees cannot be part of that buffer due to utility conflicts, include wider bands or more densely planted zones of understory plantings.
- Calm traffic by introducing trees and understory landscaping in the median and on the sides of the street that will make the street appear narrower.
- Create a streetscape with visual consistency and presence that will reinforce the impression of a contiguous, mixed-use district.
- Consider making deliberate changes in tree selection, plant choices and/or patterning in specific locations like gateways or village centers in order to differentiate these areas from the general corridor.



- Invest in the construction of a high quality streetscape system with extensive landscaping, so that merchants gain confidence in the County's commitment to the area, and are more likely to make upgrades to their properties in the corridor.
- Provide shade along sidewalks and in public spaces in order to encourage use.
- Reinforce the identity of the corridor as part of the South Valley by the extensive use of large deciduous trees in the landscape wherever possible.
- Consider use of agricultural plants and/or patterns in the landscape design to reinforce the corridor's ties to local agriculture and agriculture traditions.
- Consider use of plant materials that may have cultural significance to South Valley residents and merchants.
- Screen modern necessities like parking lots, utility structures and dump-

sters with landscaping in order to reduce visual clutter and increase the visual impact of trees and landscape within the corridor. Landscape around PNM fixtures shall follow PNM's Electric Service Guide.

Low Impact Development

- Use plant material that will function and thrive in low impact development installations such as filter strips, bio swales, water harvesting and detention areas, and permeable paving zones.
- Consider extensive use of native or regionally adapted plant materials that will thrive in local conditions with less fertilizer, water and maintenance.
- Consider extensive use of native plant materials that will support native pollinators.



Bollards provide both safety and image along walkways, crosswalks and intersections.



Lighting defines a street edge, and a transition for pedestrians.



Lighting can be combined with decorative street banner

Lighting

Lighting in the public right-of-way needs to address pedestrian needs as well as vehicular needs. Safety is the top priority. Additionally, economic redevelopment efforts will be served by introducing pedestrian scale lighting to create pedestrian-friendly corridors along the street. This lighting helps to visually separate the sidewalk from the street, improve visibility and the sense of safety for pedestrians.

- Introduce pedestrian-scale pole lighting along sidewalks to provide better visibility along the walks and at curb cuts.
- Use pedestrian-scale pole lighting to reinforce the separation of the sidewalk from traffic.
- Evaluate current street lighting design and infill opportunities and revise

- lighting as necessary to meet current roadway standards.
- Provide consistent lighting levels and reduce glare at intersections in order to increase visibility.
- Use pedestrian-scale lighting to define pedestrian zones and provide comfortable, safe places for people.
- Select one type of lamp that will provide good color rendition (metal halide or LED with filters), and use this lamp for all corridor lighting.
- Design a system of coordinated light poles and fixtures that complement the historic character of the corridor and reduce visual clutter.
- The system of coordinated light poles and fixtures shall be night sky/dark sky compliant.
- Utilize high efficiency lamps and durable poles and fixtures to reduce



Design Concept Sketch, showing gateway elements on Bridge looking east across the river.



*top: Chevy on Stick, San Mateo and Gibson
bottom: National Hispanic Cultural Center*

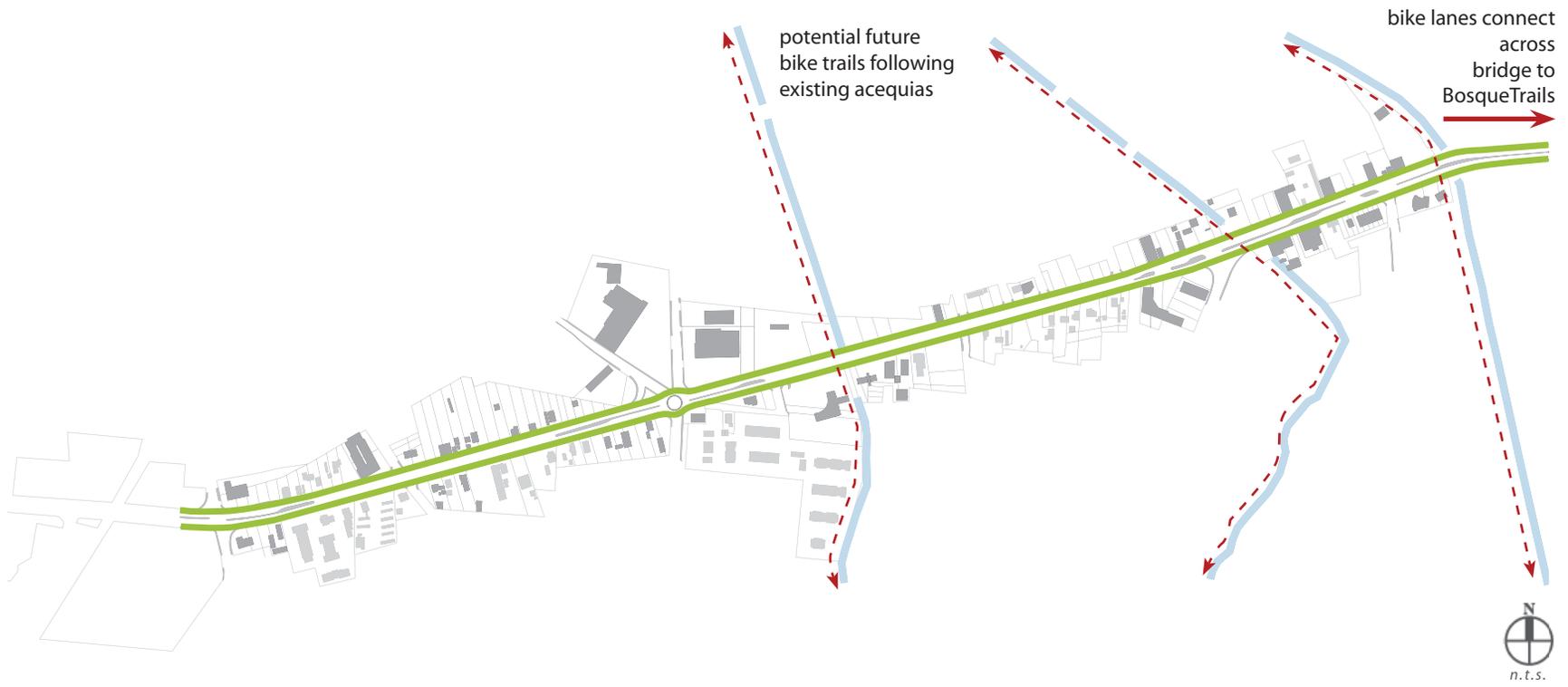
Gateway Features

In this plan, gateways have the potential to represent the physical features that define the community – both its culture and its geographical boundary. Sited at the eastern and western edge of the overlay boundary, a gateway should create a sense of arrival. Additionally, they should work in concert with the existing character and natural and built environment of the area – the Bosque, the historical bridge, the agriculture, the architecture. The design of the Bridge Boulevard gateways should reflect the unique history and character of the area, and spur economic development.

- The gateway designs should celebrate the history and character of the

corridor.

- The gateways should communicate a sense of place and identity for the corridor.
- Signage, landscaping, art and lighting should be used in an integrated manner to communicate a sense of arrival.
- Signage, landscaping and lighting design should comply with the policies stated herein.
- The design and placement of the gateways should be coordinated with other elements of the streetscape to reduce visual clutter.
- The design and placement of the gateways should be coordinated with vehicular and pedestrian circulation (trails, sidewalks, crossings) to control and optimize the viewing experience.



Bikes

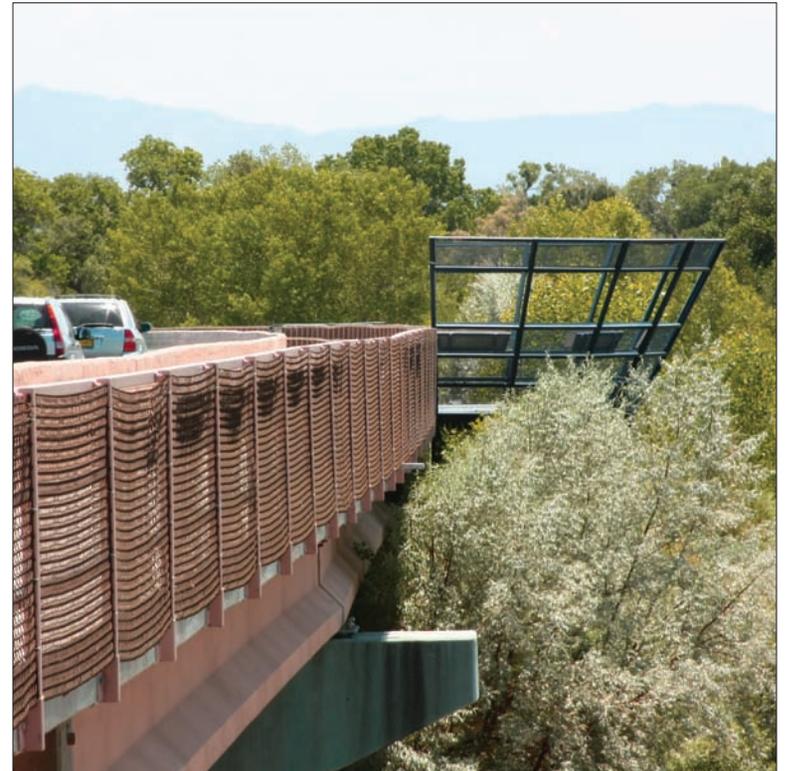
The bike lanes along Bridge shall connect with the existing regional routes that flank the Rio Grande and connect to the city-wide bicycle route system. For safety, the lanes will be signed and painted for high visibility. These lanes will provide both commuter and recreational opportunities and will serve as a community amenity.



Bike lane demarked by solid color and white stripe for increased awareness and safety.

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Corridor Design Standards



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Crosswalks will respond to the following criteria:

1. Crosswalks shall be marked by striping and/or textured/ colored pavement for high visibility.
2. Raised medians shall be used for pedestrian refuges in all crosswalks with cut-throughs for wheelchairs.
3. Pedestrian countdown signals that are audible and visual shall be installed and signal buttons shall be located at the appropriate height next to the ramp landing. Signals shall be timed for children, seniors, and the disabled.
4. Two-stage pedestrian crossings with signals and/or beacons shall be used at all mid-block locations.
5. Crosswalks shall be placed before bus stops so pedestrians are visible to motorists. Bus stops may consist of a sign and bench, or in areas of higher use, a shelter is appropriate.
6. Pedestrian lighting shall be installed along sidewalks and all crosswalks shall be well lit.
7. Pedestrian and accessible facilities are to be constructed according to AASHTO "Guide for Planning, Design, and Operation of Pedestrian Facilities."
8. Consolidate driveways and reduce the number of access points by developing shared access between properties.
9. Bus shelters require a 5 ft. wide and 8 ft. deep hard surface landing per ADA. ABQ Ride requires an 8 ft. wide by 12 ft. deep pad for each bus shelter.
10. All ramps shall be designed to ADA standards and be in line with crosswalks; two ramps at each corner are recommended.
11. All public art and site furnishings will be coordinated with existing bus stops to accentuate the wayfinding and pedestrian experience.

Bike Lanes

Despite the heavy traffic on Bridge, bikes and bike culture are important elements of daily life and contribute to a multi-modal solution to regional transportation. To properly promote and support safe biking, good practices such as those detailed below should be followed.

1. Bike lanes need to be properly defined with a high contrast stripe. Ideally the lane is painted a solid color to distinguish the extents of the bike lane.
2. Bike lanes need to be marked with proper signage, both within the lane on the ground and with posted street signs.
3. Bike lanes need to be properly coordinated and marked at right turn lanes.
4. Short term bike parking (ranging from a piece of street furniture, to a standard bike rack to a bike locker) needs to be located throughout the length of the boulevard to accommodate visitors and customers to the area.
5. Short term bike parking needs to be visible, secure, well lit, unimpeded by stationary objects and easily accessible.
6. Bicycle facilities are to be constructed according to AASHTO "Guide for the Development of Bicycle Facilities."

Landscape

1. Landscape requirements shall follow Section 19 of the Bernalillo County Zoning Code and the standards in the Isleta Boulevard Village Centers Sector Development Plan.
2. All new landscaping and development shall incorporate low impact storm water drainage techniques.



Street section looking west, showing corridor improvements



Development Design Policy



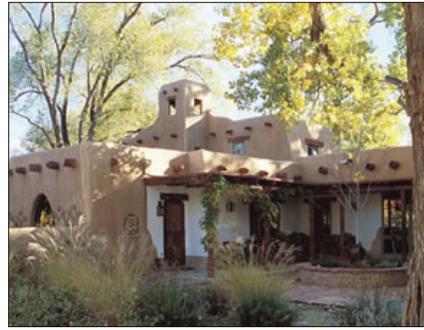
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An example of a contemporary southwestern style.



An example of animating a facade with a pergola.



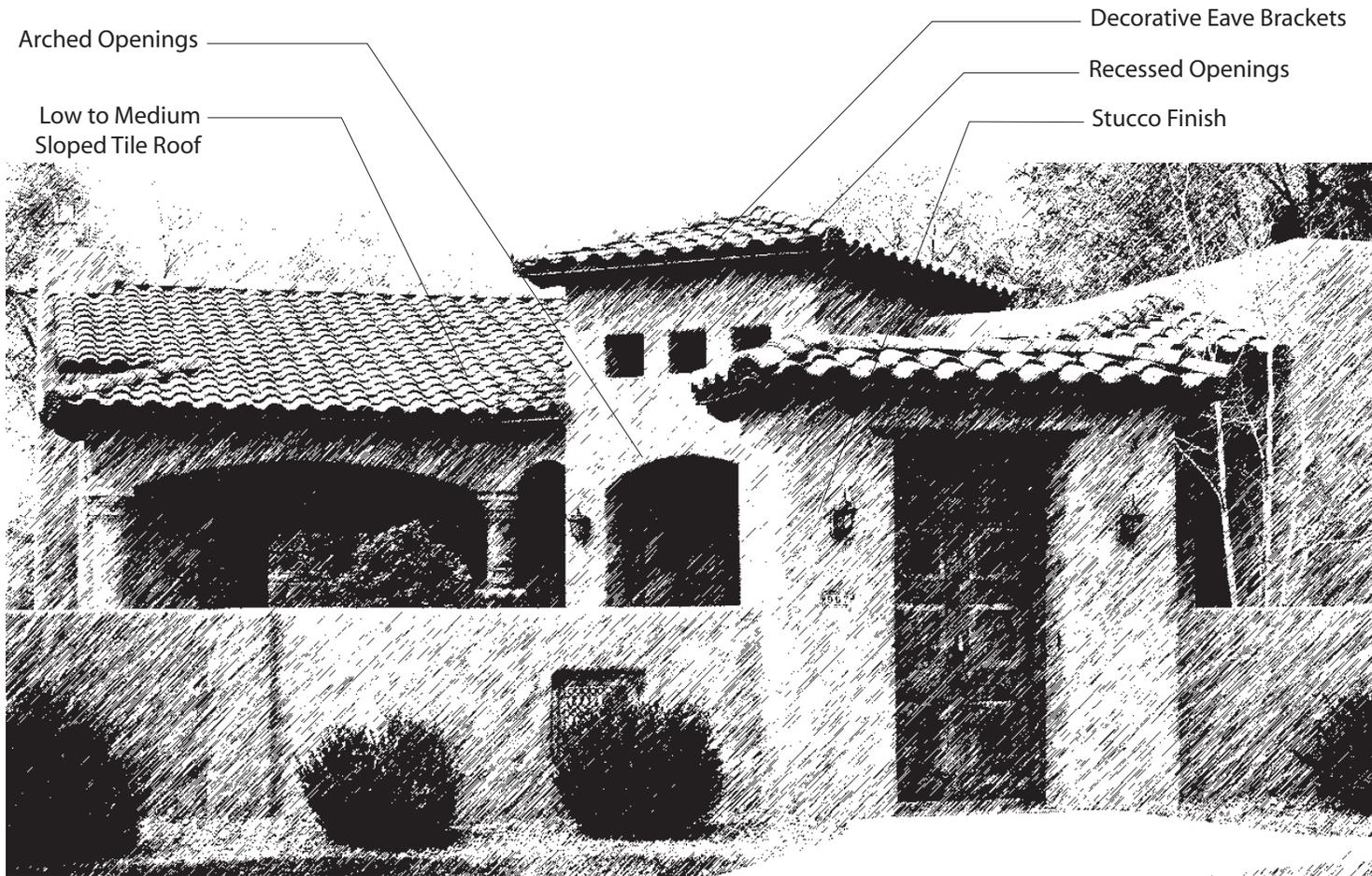
An example of pueblo style.

General Building Standards

Building on the heritage of the South Valley, Bridge Boulevard can strengthen its identity by applying a consistent attitude toward its built environment. The historic legacy of the area provides a critical path towards restoring a cohesive identity to Bridge. It is the intention of these guidelines that the design of new structures, additions and renovations should be responsive to and informed by the historical context and style of the South Valley.

The Standards at the end of this section are intended to outline an understanding of the specific objectives and design intentions that contribute to: (1) the qualities of the physical environment, (2) a unity of visual character, and (3) a unique “sense of place”.

Essential design considerations include basic massing (typically large areas of both solids and voids), proportions and materials, window fenestration, sun protection, and the incorporation of patios and other transitional exterior spaces at building entries.

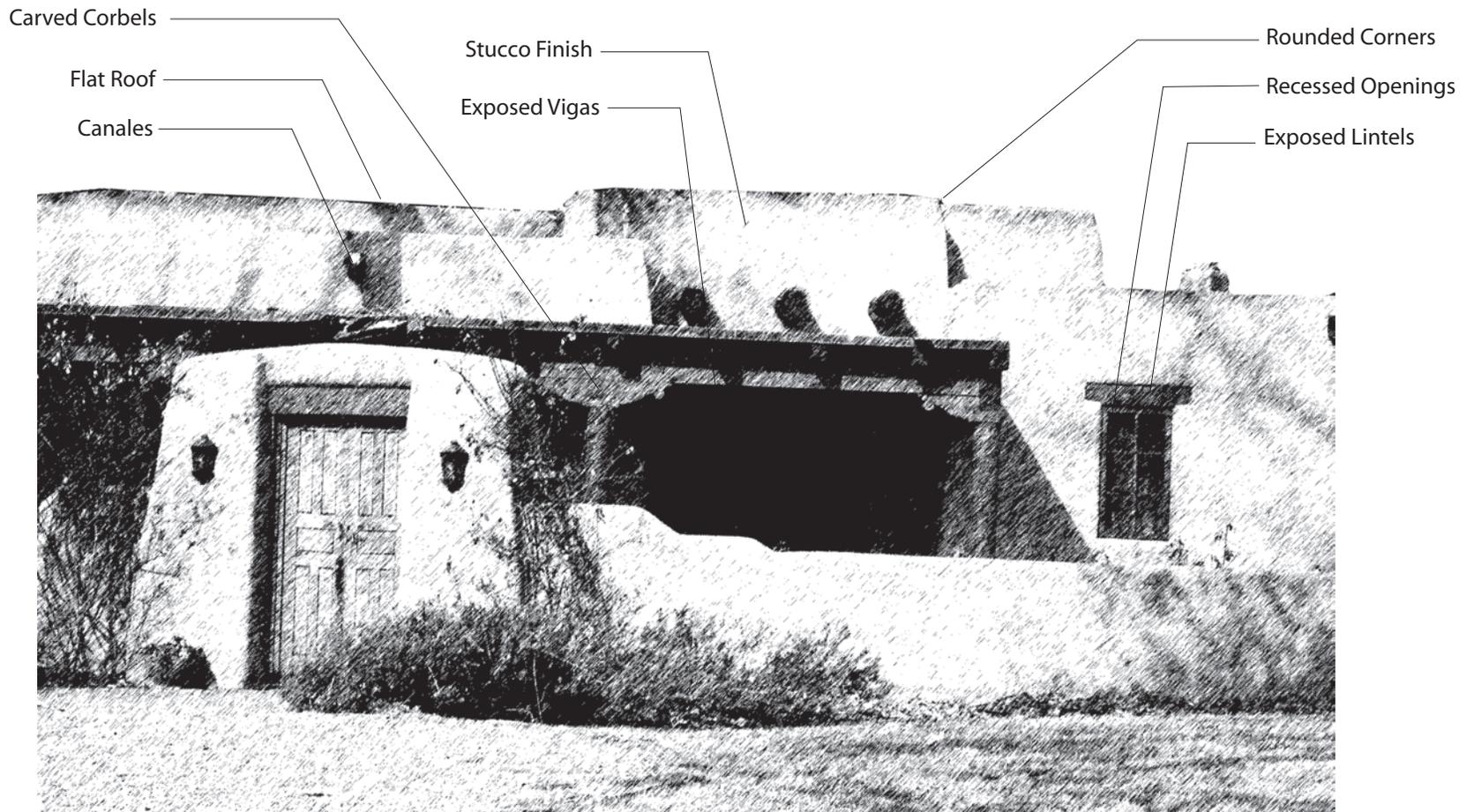


Architectures Styles

New construction and renovations should be organized and consistent in their design. Designs should reflect one of the following styles:

Spanish Colonial

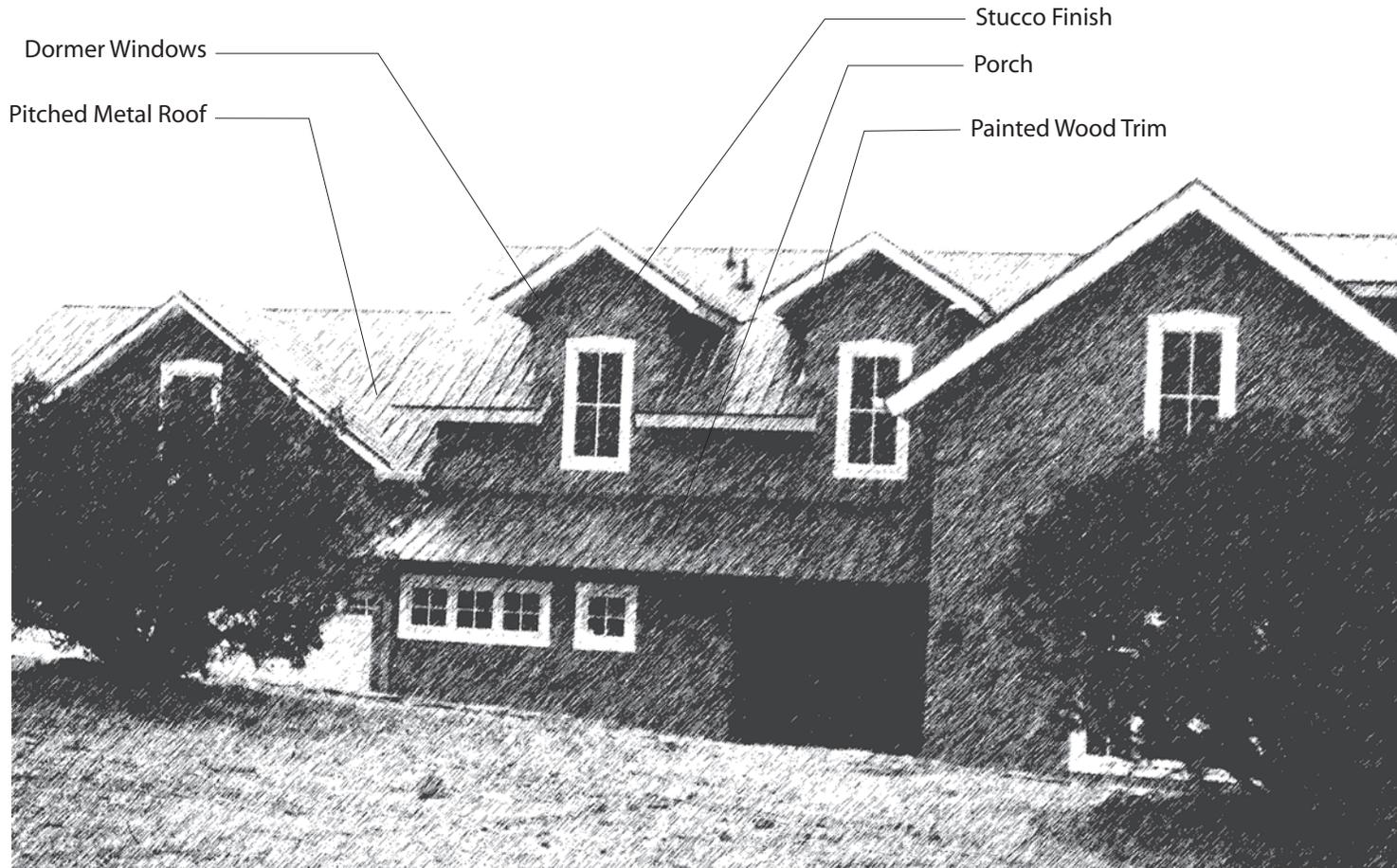
The style is marked by the use of smooth plaster (stucco) walls with low-pitched clay tile or flat roofs with rounded, sometimes tiled parapets. Other characteristics typically include small porches or balconies, wood casement or tall, double-hung windows, canvas awnings, and decorative iron trim.



Architectures Styles

Pueblo Revival

The style most strongly exhibits the traditional adobe look of New Mexico. There are rounded corners, irregular parapets, and thick, tapered walls. Walls are usually stuccoed and colored in earth tones. Multi-story buildings usually employ stepped massing similar to that seen at Taos Pueblo. Roofs are always flat. Portales can be used and expressed with wood posts and beams. Other details include vigas, canales and exposed wood lintels above the windows.

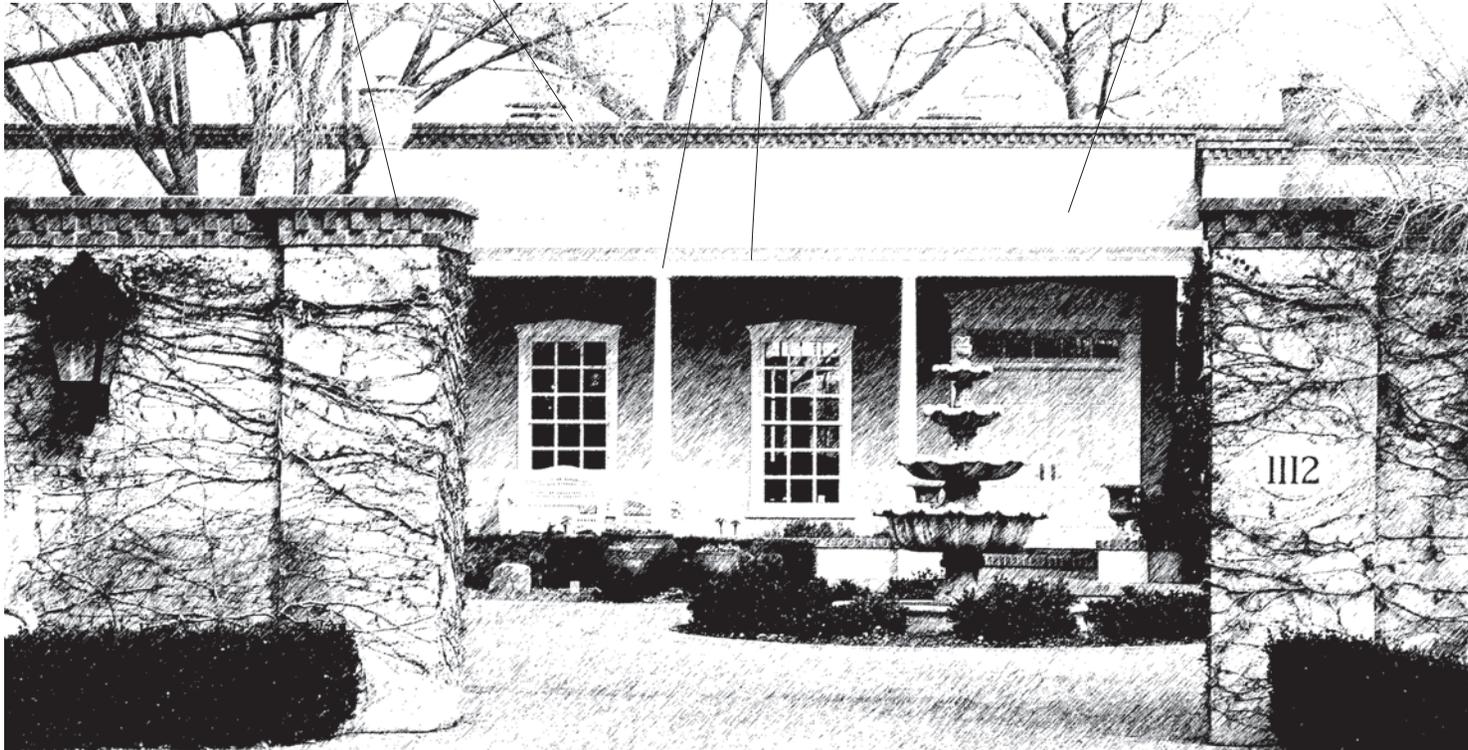


Architectures Styles

Northern New Mexico

This style is distinguished by its pitched or hipped roofs – often with a raised seam metal roof at 45 degrees. Details include porches with deep portales, decorative wood-work and post and corbels.

Flat Roof
 Brick Coping at Parapets
 Portal w/ square columns
 Painted Trim
 Stucco Finish

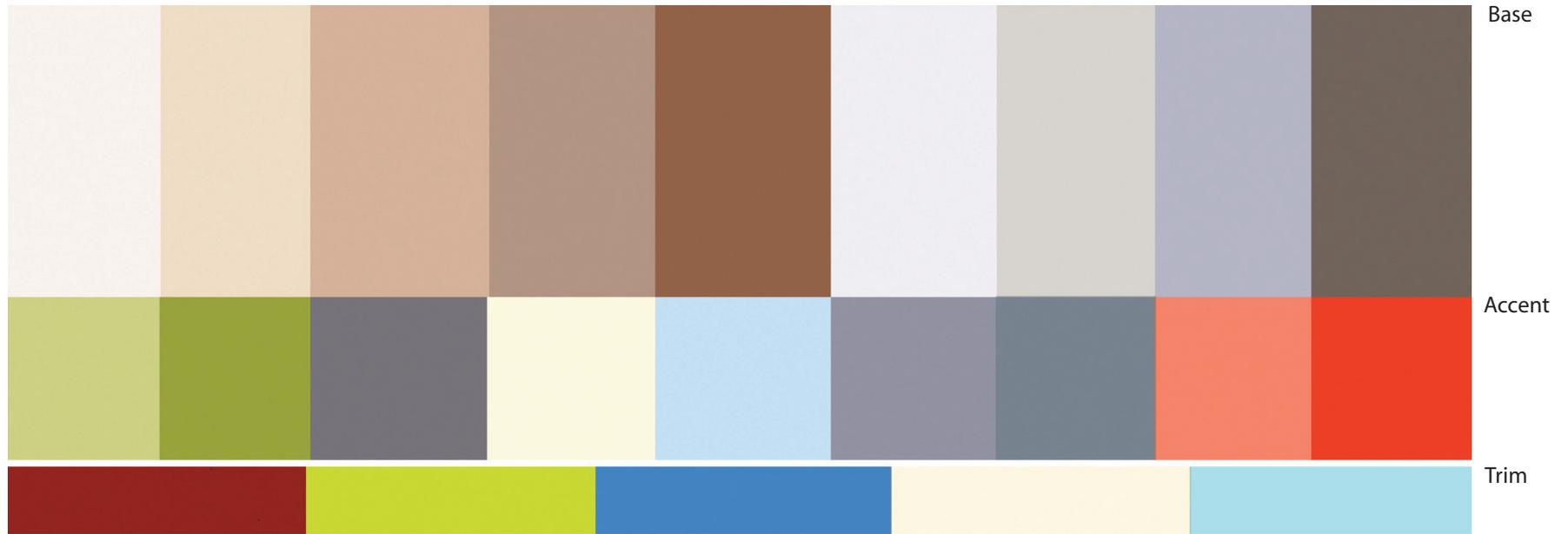


Architectures Styles

Territorial

This style is marked by flat roofs with brick lined parapets, multi-paned windows and side lights at the main entry. Other details include pedimented lintels over doors and windows and portales with square posts.

6



Color is one of the easiest ways to upgrade structures and contribute to the visual cohesiveness of the corridor. The spirit of the palette is to show case the unique character of the area that draws on both historical and cultural precedents. A monochromatic uniformity is not the intention. However, by controlling the use of more saturated colors and introducing some neutral base colors, the palette can unite the structures on Bridge Blvd. and still allow for identity.

Exterior Color Palette

Color matching the above palette to a paint or stucco manufacturer is acceptable. The color palettes shown represent a recommended range of appropriate colors and are based on the historical palette of the area.

Base color denotes the color that would cover the main body of your structure. Excluded from the body are doors, window and door trim, exposed lintels, columns, posts, exposed structural joists or beams, decorative metal, brick parapet coping, glass, and roofing material.

Accent color denotes a complimentary color to the base color and is used to emphasize certain architectural features (like an inset porch), volumes

and planes including a wall. The accent color should be less than 30% of the overall base color. Exceptions to this can be made on a case by case basis.

Trim color denotes a stronger color used exclusively as an outlining device. Trim includes exposed window and door framing and exposed structural members like columns, joists, and lintels. A brighter trim or accent color may be used to paint a door as a feature item. Accent colors may be used as trim colors. Complimentary base colors may be used as accent colors. At a minimum, window and door trim shall be a different color from the base.



A low stucco wall helps provide a buffer from a parking area, and also compliments the architectural style of the surrounding buildings.



Close up of a Coyote Fence



Color and layering breaks up long walls. Curved forms also create visual interest along a pedestrian corridor.

Walls + Fences

Walls and fences work in three ways. They can be a compliment or extension of a building. They can also provide security to and screening of a property and define its boundaries. They can contribute to the street edge and character. When used to define a property edge, there needs to be a balance between security and maintaining a pedestrian friendly, visually appealing street view. Walls and fences are not required, but when used, they should respond to the environment both inside and outside the property. With the right materials, walls and fences provide a wonderful opportunity to bring interest and definition to a property.

If a wall is used, vines or other landscaping should be used to soften the appearance of the wall from the street.

A traditional adobe wall accented with trumpet vine





Low walls and landscaping along a sidewalk are used to define public and private areas.



Shade trees and a low wall help define a pedestrian zone.



Directing storm water into landscape areas helps reduce the use of potable water for irrigation.

Landscaping

Landscaping of private property along Bridge Boulevard will have a significant impact on the character of the corridor. The ability to plant large trees within the public right-of-way on the south side of Bridge Boulevard is limited due to the existence of overhead power lines. This is unfortunate because trees serve many design needs – shade, scale, color, mass, edges. Policies that encourage planting large trees on private property along both sides of the street will reinforce the visual balance and continuity that is desirable along the corridor. Additionally, shading elements, such as canopies, awnings, and arcades are encouraged over outdoor seating and over adjacent sidewalk areas to provide a more comfortable pedestrian environment.

Native plants reduce the use of potable water, and also reduce the need to use chemical fertilizers and herbicides. Many xeric plants are also very sculptural and provide contrast within the landscape.





Using compacted crusher fines within the parking spaces creates a permeable solution for large parking areas.



Planted areas around parking helps reduce the heat island effect.



Permeable parking area can transition easily into a paved walkway with a landscape buffer.

Off-street Parking

The design of parking within the Corridor Plan zone is a key consideration in creating a successful corridor. Adequate and convenient parking must be provided in a manner that does not make parking the predominant feature of an individual site.

Where right-of-way permits (90 feet between Isleta Boulevard and La Vega Drive), landscaped curb extensions will be constructed to protect existing on-street parking areas. Bike lanes will be clearly marked on the inside of the parallel parking aisle.



Example of wood, pedestrian level sign that contributes to sense of character and place.



Example of a window sign



Example of an awning sign

Signage

The size and type of signage plays an important role in communicating to the intended audience. Large signs reflect a higher speed auto-oriented scale, while smaller signs with a higher degree of detail communicate a sense of welcome and interest to pedestrians.

Primary entries should be clearly expressed by building massing, and recessed or framed by sheltering elements such as awnings, arcades, or porches. Secondary entries should be treated in a similar, but lesser manner.

Inviting and accessible exterior spaces adjacent to buildings are encouraged. These spaces should be physically and visually linked to primary entries and parking locations and be located either in front of a building (acting as a buffer to the street) or to the side of a building. Exterior spaces should be scaled relative to the building. Examples of this type of space are courtyards, patios, gardens with seating, plazas, and covered porches.



Example of selective lighting that highlights an architectural feature



Example of lighting that emphasizes entry and complies with Dark Sky practices.



Example of civic lighting

Lighting

The purpose of this section is to limit the amount of light that is allowed to be transmitted beyond the area intended to be illuminated. This will allow for adequate lighting and security of private property without producing glare or light pollution into public spaces or adjacent properties.

Lighting of private property along Bridge Boulevard can have a significant impact on the character of the corridor. Artful lighting of buildings, building entrances, patios, and parking areas will help focus attention on businesses, differentiate sections of the corridor, and highlight the relatively small scale and fine patterning of development along the corridor.

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Development Ordinance Requirements



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Zoning

This Corridor Plan does not alter the zoning for properties along Bridge Boulevard. Zoning will continue to follow existing designations and requirements. Parcels that adjoin Bridge Boulevard and are also within the Isleta Boulevard and Village Centers Sector Development Plan zone (from the Eastside Drain to Hartline Road) shall remain consistent with the SD-IVC (Isleta Boulevard Village Center Zone) designation that is part of the Isleta Boulevard Sector Development Plan. Properties located within the Goff Village Center and Five Points Village Cener may apply for Isleta Boulevard Village Center Zone.

Implementation

Thresholds for application: Any addition to a building or structure that is 200 square feet in area or greater and all new development shall necessitate compliance with the Design Requirements listed below. Any project that requires a zone change or a Special Use Permit is also subject to these requirements.

Applicants will follow standard County requirements and procedures for site plan approvals.

In addition, applicants will provide the following:

1. Color samples of base, trim and accent colors.
2. Specifications for proposed exterior lighting.
3. Specifications for proposed signage.

With regard to color, existing non-conforming properties shall have one year from the approval date of this Corridor Plan Plan to conform to the Design Requirements.

General Building Standards:

The following standards are intended to contribute to: (1) the quality of the physical environment, (2) a unity of visual character, and (3) a unique “sense of place”. Essential design considerations include basic massing (typically large areas of both solids and voids), proportions and materials, window fenestration, sun protection, and the incorporation of patios and other transitional exterior spaces at building entries.

1. Maximum building height shall be 26 feet or 2 ½ stories in height, except as provided in the Supplementary Height and Area Regulations of the Comprehensive Zoning Ordinance of Bernalillo County.

2. All street-facing walls of a building shall be articulated with consistent style and materials. Exterior walls shall contain windows, punched openings, change in height or plane, awnings or vertical posts every 15 feet. In no case shall any façade consist of non-articulated blank walls longer than 15 feet.



3. Material changes shall occur at inside corners, (versus outside corners) of structures.
4. All mechanical equipment and meters shall be located to minimize visual impacts from streets, sidewalks and other public spaces. Rooftop mechanical equipment shall be screened from view when standing on the adjoining sidewalk.

Setbacks

1. Reduced setbacks result in a more active and interesting pedestrian realm. Allowable setbacks for zoning within the Corridor Plan area are as follows:

Front (Maximum): 20 ft

Rear (Minimum): 5 ft

Side (Minimum): 0 ft

2. There is no minimum required front-yard setback for structures with ground-floor commercial uses. Where a front yard setback is provided, it shall be landscaped.
3. Corner lots shall have a maximum setback on the street side of ten feet in addition to the appropriate front setback.
4. Attached dwellings shall maintain a separation of no less than ten feet between structures (a structure may contain multiple dwellings).
5. Parking shall not be permitted in front setbacks.
6. PNM has a 10' Public Utility easement along portions of the south side of Bridge Boulevard.

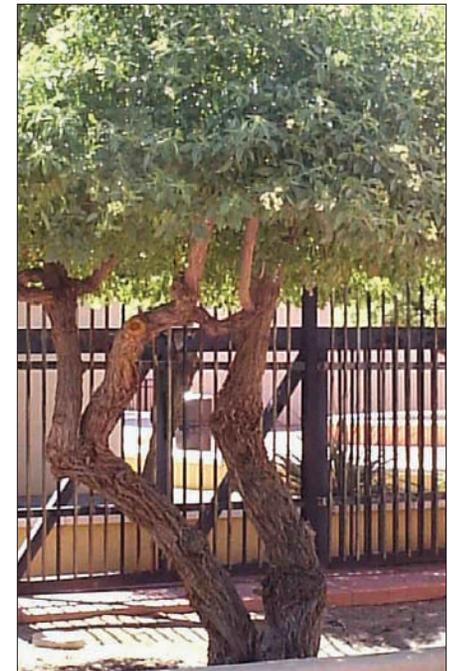
Color

Applicants will be required to provide color samples at the time of application for building permit. The color samples shall be a minimum of three inches square and shall be consistent with the proposed building colors, including trim, accents, and primary building colors.

1. Allowable colors are shown on page 44.
2. Colors on block and stucco walls visible from the Bridge Boulevard right-of-way shall complement and/or coordinate with the main structure.
3. Color changes shall occur only at inside corners or at pilasters.
4. Highly saturated colors shall only be used as accent and/or trim colors (see colors allowed on page 48).

Walls and Fences

1. All walls visible from the Bridge Boulevard right-of-way shall have a finished appearance (stuccoed, painted, or cladded).
2. Walls and fences used for screening purposes within the Corridor Plan are not to exceed six (6) feet in height. Four (4) feet or less is preferred. Trellises, arbors, and semi open structures are acceptable substitutions for solid walls if landscaping is used to enhance the visual buffer.



3. Walls and fences buffering residential uses from non-residential uses should take on the character of the residential use.
4. Where visible from Bridge Boulevard, chain link fencing, chain link with slats, razor wire or concertina wire is prohibited.
5. For walls, allowable material choices include burnished, split face or colored concrete masonry units (cmu), plain cmu with a stucco finish, stone or adobe bricks.
6. For fencing, allowable materials include the following:
 - a. Wood – coyote fences, peeled pole fences and picket fences
 - b. Metal – tubular steel, wrought iron, and metal panel. For colors other than flat black, fencing shall follow the allowable colors shown on page 44.

Landscape

Landscape shall comply with the Bernalillo County Code and the Isleta Boulevard and Village Centers Sector Development Plan. The following are additional requirements.

1. New development within the Corridor Plan shall be required to surpass the number of required trees per current code by 10%.
2. Landscaping, low walls and patios shall be used to define the transition from the public right-of-way onto private property. Within the PNM public utility easement on the south side of Bridge Boulevard, shrubs shall be used in lieu of trees (see exhibit on page 28).
3. Landscape shall be used in front of or in lieu of screen walls (where feasible) to highlight landscaping, obscure graffiti-prone surfaces and provide visual interest in the pedestrian zones of the corridor.
4. Water harvesting (at a minimum, directing water to landscaped areas) is required in off-street parking areas with more than 150 parking stalls.
5. The landscape design shall be water conserving in the broadest sense - shading buildings and parking areas and reducing energy use; utilizing drip irrigation to eliminate overspray and reduce evaporation; using drought tolerant and native species that flourish in local conditions; and restricting the use of spray irrigation unless it is for turf or native seed areas.
6. Flood irrigation is prohibited in the corridor unless it is for agricultural uses.

Parking

The design of parking within the Corridor Plan is a key consideration in creating a successful corridor. Adequate and convenient parking must be provided in a manner that does not make parking the predominant feature of an individual site.

1. Required parking ratios are the same as in the Isleta Boulevard and Village Centers Sector Development Plan (see Section Four, Parking Requirements, of the Sector Plan). Handicapped parking requirements follow Bernalillo County Zoning Code. Bicycle parking shall be provided at one space per 25 parking stalls.
2. Businesses that are within 1,000 feet of a bus stop shall be eligible for a 20% reduction in overall parking requirements.
3. Off-street surface parking shall be located at the rear and sides of a building relative to Bridge Boulevard. Parking shall not be permitted between a building and the street. On corner lots, parking is allowed along the side street.



4. Parking areas at the side of a building shall not constitute more than 50% of the street frontage and should be screened from view from the public right-of-way.
5. Parking areas over 150 stalls shall be divided into smaller sub-areas by a building, internal landscaped street or shaded landscaped pedestrian way with trees. Drainage shall be directed to planting areas to maximize percolation and reduce the need for irrigation.
6. Off-street parking areas shall have a minimum landscape coverage of 15%. Each planted area should not be less than 25 square feet. Drought-tolerant or native plants shall be used to reduce watering needs.
7. Loading areas shall be separated from automobile parking and screened from view from the Bridge Boulevard right-of-way.
8. Surface parking areas visible from Bridge Boulevard shall be screened with landscaping, low walls, or planters that are located behind the sidewalk and in front of the parking area. This landscaping shall be a minimum of 10 feet wide, or a minimum of 6 feet wide with a minimum 4 foot high screen wall or trellis.
9. Parking lots on adjoining properties shall be interconnected whenever possible to allow pedestrians to link trips by parking their car in one lot and making several trips on foot, and to offer drivers more flexible parking choices.
10. For retail uses over 50,000 square feet, parking can be located between the building and the street provided that a minimum of 50% of the street frontage of the parcel is occupied by building frontage within the maximum front setback.
11. Pedestrian walkways/ADA access shall be provided between bus stops, public sidewalks and building entrances.
12. Pedestrian walkways shall be provided between parking aisles to building entrances. Walkways are to be clearly demarcated from the ADA parking spaces to building entrances.
13. Drive through aisles and drive-up windows are to be placed at the rear or sides of all buildings, never in the front.

Signage

1. Freestanding signs shall be limited to ten (10) feet in height and designed as monument signs with an enclosed base. The outer edge of a wall sign should be allowed to protrude up to one foot over the property line into the public right-of-way, provided the bottom edge of the sign is eight (8) feet or more above the curb or sidewalk grade.
2. Not more than one sign shall be permitted for any one premises with street frontage of 50 feet or less.
3. The total area of any one sign face shall not exceed 32 square feet. The total aggregate of all faces of signs or combination of signs allowed for the property on which the use is located should not exceed 130 square feet of sign area. Businesses fronting on more than one street shall be allowed additional square footage of sign area to the extent of 50 percent of that allowed for its main street frontage.
4. Illuminated signs, including, but not limited to, illuminated clocks, thermometers, and illuminated signs within a building, shall be so located as to not shine directly into adjacent residential property.



5. One awning valence sign per street level business façade, not exceeding 50% of the awning valence area and centered on the awning valence, shall be permitted. No additional lighting for the awning valence shall be permitted.
6. One shingle or suspended sign per street level business with a maximum of four (4) square feet of sign area shall be allowed. Shingle and suspended signs shall be of wood or material simulating wood, suspended from the underside of a pedestrian canopy or awning directly adjacent to the business identified on the sign. Shingle and suspended signs shall be oriented perpendicular to the adjacent wall of the building being identified and attached in a manner that prevents swinging. Height above grade shall be consistent with Bernalillo County code.
7. Two street-level window signs shall be permitted per street level business per building façade located on or adjacent to the inner window surface and directly facing a street. Signs shall not exceed 15% of the window area or nine square feet, whichever is smaller.
8. A maximum of one window sign per upper floor business per building façade shall be allowed, with each window sign not to exceed nine square feet each and not directly illuminated. Colors shall match or complement the street level display window sign of the same building.
9. Wall mounted signage shall be consistent with Bernalillo County zoning code and shall conform to the color palette shown on page 44.
10. Flashing, blinking, or revolving signs and signs with audible devices shall be prohibited.

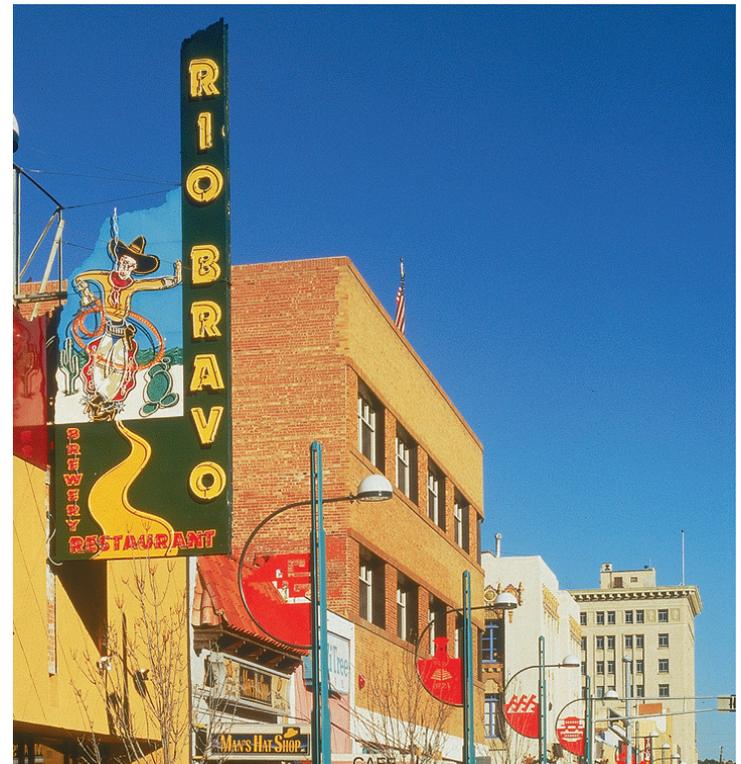
Lighting

1. Pole heights shall be a maximum of sixteen (16) feet high.
2. All lighting fixtures shall be night sky/dark sky compliant.
3. Lighting shall be located so that it does not shine off of the property.
4. Uplighting is prohibited.
5. Lamps shall be incandescent, metal halide or light-emitting diodes (LED).



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Economic Incentives



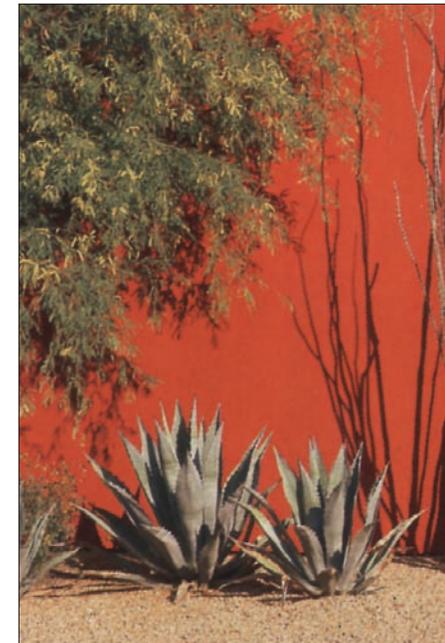
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Goals and Strategies

The County has initiated a number of incentives designed to help small businesses in the County. These initiatives are a result of overall goals articulated in the Albuquerque/Bernalillo County Comprehensive Plan (Comp Plan). The Comp Plan includes the following economic development strategies that are shown in italics below. Each strategy is followed by a short explanation of its relevance to Bridge Boulevard.

- *Provide a diversified economic base by offering incentives to local employers to help retain and expand employment activities.* With the large number of small businesses along Bridge Boulevard, the County has the opportunity to offer incentives designed to help businesses along Bridge Boulevard invest more, expand their operations, and retain employees.
- *Develop special strategies for correcting problems of disinvestment.* Bridge Boulevard has virtually no businesses that are regional or national in nature. The corridor needs a larger pool of investment capital to help fund new and existing development.
- *Promote the concentration of employment in activity centers in an effort to balance jobs with housing.* One of the main challenges for Bridge Boulevard is the disconnect between jobs and housing. Most of the traffic is due to people commuting to jobs in locations other than Bridge Boulevard. Generating more employment along Bridge Boulevard could help alleviate some traffic associated with commuting to jobs.

Part of economic development is creating a more visually attractive corridor that helps promote private investment. Bernalillo County is committed to funding improvements along Bridge Boulevard for specific elements such as lighting, signage, landscaping, and gateways that help to create a more attractive and safe environment. This public investment needs to be accompanied by private investment on individual properties and at the Village Centers identified along the corridor. The following page lists some incentives that can help property owners re-invest in their businesses along Bridge Boulevard.



Incentives

By identifying the Bridge Corridor as an area of emphasis certain economic development incentives can be identified that will help business to locate or expand in this area of Bernalillo County. Some of the tools available to the County are listed below.

Impact fees

The criteria for determining which projects qualify for an economic development impact fee exemption include the number and quality of jobs the project will generate and maintain, the ad valorem taxes the project will generate, environmental and community impacts, the longevity and creditworthiness of the company and whether local facilities will be leased or owned. For more information, contact the County's Economic Development Office (contact information is listed at the end of this chapter).

Property Tax rebate. Property owners along Bridge Boulevard will be eligible for a property tax rebate for up to 75% of the cost of improvements required to bring a property into compliance with the Design Requirements. Eligible expenses include items such as new landscaping, exterior painting, signage, walls, and fencing. Up to \$7500 in allowable costs will be eligible for property tax rebates. The \$7500 ceiling can be spread over three years.

Job Training Incentive Program

New Mexico has one of the most aggressive training incentive packages in the country. The Job Training Incentive Program (JTIP) funds classroom and on-the-job training for newly created jobs in expanding or relocating businesses for up to six months. The program reimburses 50 to 70 percent of employee wages and required travel expenses.

High-Wage Job Tax Credit

A bold, new incentive created in 2004 by Governor Richardson and the New Mexico Legislature, this tax credit equals 10 percent of the wages and benefits for each new economic-base job created. Qualified employers can receive the credit for up to four years.

Manufacturers Investment Tax Credit

Manufacturers may qualify for a tax credit equal to five percent of the value of qualified equipment imported to New Mexico.

New Mexico 9000

In September of 2004, NM's unique, low-cost ISO 9000 certification program won first place as most innovative state program from the Council of State Governments—WEST. Under New Mexico 9000, businesses may obtain ISO 9000 certification on a sliding scale costing \$1,000 to \$6,000 (compared with \$120,000 by conventional means).

For more information, contact the County Economic Development Office at:

Office of Economic Development

One Civic Plaza NW

Albuquerque, NM 87102

tel: 505-468-7185

email: ed@bernco.gov

